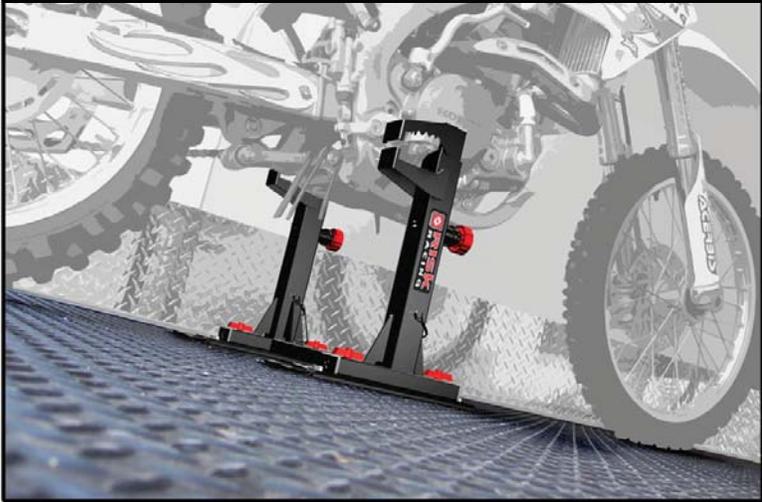


MX LOCK-N-LOAD

MOTO TRANSPORT SYSTEM



TIPS AND FREQUENTLY ASKED QUESTIONS:

Question: Is the bike stable without tie-downs or wheel chocks?

Answer: Yes, the bike is very secure and stable as you drive down the road. The front tire may move a little from left to right, but is generally held in place by the rake of the suspension. If you do find movement of the front tire, it will not affect the security of the bike.

Question: Can I leave my bike clamped in the Lock-N-Load overnight without damage to my suspension?

Answer: Yes, the Lock-N-Load is designed to put minimal pressure on your suspension. The bike sits at less than pre-load height and distributes the pressure to both front and rear suspension.

Question: Why can I wiggle the bike a little bit when I shake the handle bars or press on the seat?

Answer: The Lock-N-Load is designed to hold your bike securely as you travel down the road. When being transported, all the forces on the bike come from the trailer floor and are transferred directly to the tires and foot pegs, not the bars or the seat. You can see from the videos online that the bike travels very securely when held in this manner.

Question: Can I mount bikes close together with the Lock-N-Load?

Answer: Yes, in fact you can mount them right next to each other, shroud to shroud. In a common toy hauler or wide body trailer, you can mount 4 bikes wide by slightly staggering the handlebars as shown in the image above. You can also mount them fairly close to the trailer wall.

Question: Is the Lock-N-Load adjustable?

Answer: The Lock-N-Load is designed to accept all major, full size, **motocross specific bikes**, as well as many trail bikes that have similar bike geometry in regards to foot peg height and width. Making the product adjustable was an option but it would make the product both **weaker** and more expensive. (Suggested Bikes: CRF150 big wheel, all 125cc MX bikes through to all 450cc MX bikes)

Question: Can I mount the Lock-N-Load in the bed of my truck?

Answer: Yes, we have a truck bed accessory plate that spans the corrugation of your truck bed and mounts the Lock-N-Load in the bed of a truck. This eliminates the need for a brace on the front of your truck bed to prevent the front of the bed from bending. You can also stagger the mounting and easily put three bikes in a standard truck bed without putting one in backwards. Additionally, the Lock-N-Load is quick release from the truck bed leaving only the low profile mounting plates so you can still use your truck as a truck!

Question: Will the Lock-N-Load work with my 2-stroke?

Answer: Yes, but the large collector area of your 2-stroke exhaust may be an obstacle. Simply lower a jaw when loading the bike or simply put the bike in backwards. It will work either way.

Question: Why do the mounting plates have D-Rings?

Answer: The D-Rings have nothing to do with the Lock-N-Load. They are simply there as an additional tie-down location for other items you may have. The low-profile mounting plates are mounted to your trailer floor so they might as well be another functional part of your trailer.

TIP: Have the knobs rotated into the locked position (all the way in) before your last moto. That way, when you ride off the track, you can ride directly into the Lock-N-Load and simply step on the top jaws to lock the bike into position.

More questions? Call Risk Racing, Monday - Friday, 8-5pm EST : 704-658-9332